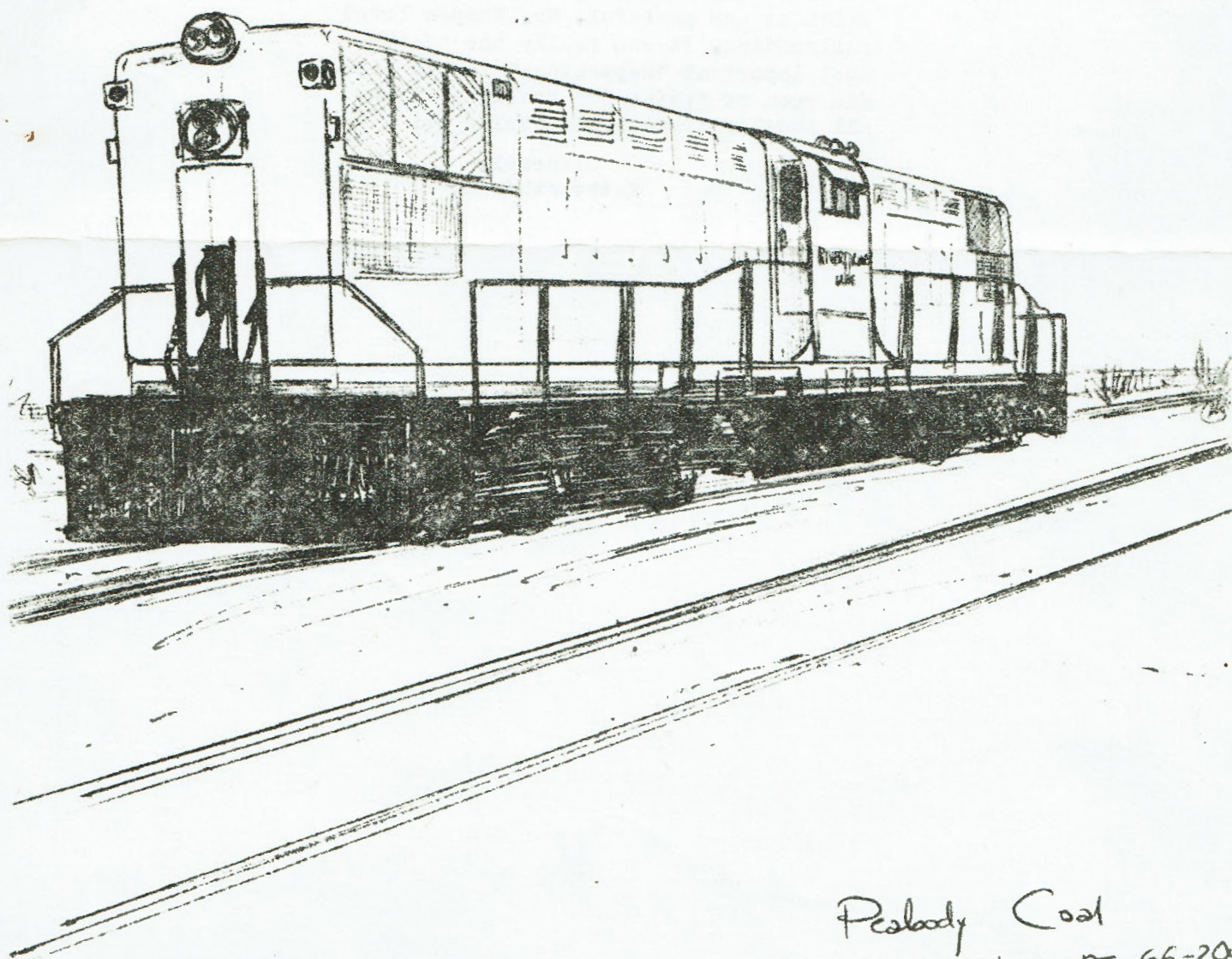


TAMR

HOTBOX

March/April Issue

TV-12



Peabody Coal
Baldwin DT-66-20

IN MEMORIAM

Dear Membership,

I am sorry to have to inform you
of the death of a TAMR member,

William E. Turpin

79 Wawona Street

San Francisco, CA 94127

He died very suddenly, and easily for
which we are grateful. Mr. Turpin loved
railroading. It was really one of the
most important "happenings" in his life.
His room of trains was really a sight
all model railroaders marveled at!

Sincerely
Kathy Atkinson

FROM THE CAB

The search is over! Yes, after some arm-twisting of you all, one of the ranks has stepped up to become the new editor, and after some discussion, Mark Tomlinson, 306 W. Main, North Manchester, IN 46962, has decided to accept the position, effective with the next issue.

I hope you will welcome Mark with a flood of articles, ads, photos, and drawings. I am also sure that he will be happy to hear from anyone who wants to help in some other way.

While I am leaving as editor, I hope to be around the TAMR for a long time yet. This job has caused my correspondence to drop off, so all of you who have been writing, please start again. Once again, please give Mark your support so that he can continue to improve the Hotbox.

ELECTION...

By action of the Executive Board, the nominations for President and Secretary have been extended to April 21, 1977. The reason the extension was granted was because only one nomination for each office was received, also some mail problems resulted in a potential nominee getting the word too late. So, if you wish to make a nomination, use the back page of your last Hotbox, or put your choices on a sheet of paper with the position you are nominating them for and send to John McGreevy, TAMR Auditor, 738 Camino Durango, 1000 Oaks, CA 91360. Remember only regular members can be nominated! If no further nominations are received, appointments to the offices mentioned will be announced in the next issue; if some are received, a ballot will be included for your vote. Remember it's your organization, you the members have the right to choose who is going to lead you and keep your records. Also, any nominations received after the April 21st. deadline will not be considered for the offices up for reelection, so nominate early!

From the Hotbox staff:

A FAREWELL TO YE OL' EDITOR...

We of the HB staff- Mark, Andy & Ed- wish to express to Tim Vermande who is stepping down from the editorship, that it has been a pleasure working with you on the Hotbox. We feel that Tim has worked hard on each and every issue and most importantly he saw to it that it was always out on time. We hate to see you go Tim but we realize that graduate school is very demanding and that you would not be able to devote the proper time necessary to put out this publication. So, Tim we bid you farewell.

As for the incoming editor, Mark Tomlinson, we are behind you one hundred percent and we feel that you can continue in Tim's steps to further improve the Hotbox!

Unlike his elderly equivalent at another magazine, the Hotbox editor rarely is seen writing much besides editorials. And when he does write, his bleeding-heart liberalism shows clear thru in the way that he finds something to write about besides the old, rusty steam engines in his old, decrepit home town. So here the editor takes up his pen and his column to relate a strange experience that occurred to him and a couple of friends who were home over Christmas...

SILVER STREAK: AN UNREVIEW OF A MOVIE
by "Otherwise Anonymous"

By now you have certainly heard of Silver Streak, if not seen it or read reviews of it in your local rags. Ah well, I have seen it too ignoring the local version of a critic and listening to those whose opinions I value more. My first regret, as a good railfan, was the attention given to the stations and not the locomotives, ex-CP's skillfully relettered "AmRoad".

To be serious, the movie may be a little unbelievable--who falls out of a train into a Canadian River, tries to hijack a Kansas sheriff's car, and gets landed by the Illinois State Police for his efforts? Also, where else but in Hollywood could a runaway train pass thru a CP reefer yard near Montreal, run downhill (enough to gain speed) under Roosevelt Road in Chicago (which is a flat stretch of track), see the towerman find his boss in a restaurant and then proceed to watch as the train runs thru Roosevelt Road, into a shed at LaSalle Street Station in Chicago (1 mile to the right), thru the bumper post there, crash the wall at Montreal's central station and then end up going thru a bookstore in Toronto Union, thru another wall to stop in of all places on the floor of Union Station in Chicago. This was indeed fortunate because they were afraid it might wreck Marshall Fields (which is 2 miles north of La Salle Street, and anyway Chicago's Union Station is double ended in the first place) if it hadn't stopped.? These questions you will have to answer for yourself! However, it must be the boy meets--loses--re gains girl basis of the plot that puts so much life into this picture. It might also possibly teach you to be sure the door you're opening in your roomette goes to the closet--and if it doesn't, at least don't get involved with a girl whose boss is unpopular with unscrupulous gangsters.

To close what I haven't started yet, go see the movie. This review will certainly make more sense than. Take your girlfriend, it isn't entirely a railroad movie. And consider what a happy lot all the Silver Streak are; compared to what you hear about how it really is!!!!

SILVER STREAK:
AN UNREVIEW OF A MOVIE

THE GREAT RAILROAD CAR REWARD!!

Did you ever wonder what happens to those old worn out cars on the "real" railroads? Do you ever wonder what you can do with those cars on your pike that just have no "life" left in them? If you have this article might be just what you have been looking for!

What really happens to those cars which are no longer "useful" to the railroad any more? They certainly don't give them gold watches, a pat on the back and retire them with a nice pension after many faithful years of service! Why no!, they scrap them, of course. Or rather they sell them to salvage yards which scrap them then in turn sell the scrap for whatever they can get. So then, this article deals with salvage yards--a great place to get rid of those old cars which don't run "right" anymore.

The salvage yard on a model railroad is just about the only industry that can conduct business, while others seem to conduct it, the salvage yard really can. "How", you ask. Let's suppose you have a terrible wreck on your pike, for example; a caboose derailed on a deck bridge on the summit between two mountains and goes plummeting to zero elevation right in front of your brand new tunnel portal. You now can dispatch your trusty work train to pick up the mess before the 8:30 express train comes thru. So, you load the caboose in a few empty gons, you brought just along for that purpose, and return to the yard. The gons are then assigned to the next local going by the salvage yard and they are dropped off there to be scrapped for a good cause, of course.

For the building of a truly prototypical salvage yard, much space is needed but one can be well represented on your pike with a little "selective compression" on your part. To estimate, the bare minimum amount of space needed is a spur capable of holding two cars and enough room on one or between the sides for a medium-sized industry. Surprisingly the more oddly shaped space available the better the salvage yard will look!

One absolute essential for a salvage yard is a crane. It can be stationary model or one that can be used on the tracks. You could even lease your work train's crane to the salvage yard with an agreement that it must be made available for any on-line wrecks. This way if your pike is low on derailments you can have the crane do something, rather than having it rust away sitting on a siding.

Practically all salvage yards have a tall fence around them. This fence is to keep the scrap in and the people out, thus avoiding as much stealing as possible. This tall fence which completely surrounds the yard gives it a definite shape and except for the entrance of the tracks and the gate that can be opened to permit scrap trucks and employees to come and go, the fence has no openings. What so ever!

Salvage yards usually contain some sort of building where the boss can keep his books and the employees can keep warm in the winter months. This building was probably obtained a long time ago in a deal for some scrap where in the railroad also wanted to get rid of a no longer used building and thus sold it to the yard with the scrap as a package price.

Some of the more interesting details that are usually around salvage yards are the "no photo" signs and even sometimes nasty men to keep those pesky railfans out! An interesting little scene can be made in having a railfan climbing the fence to get a picture of that almost non-existent diesel that has been just delivered to be scrapped!

The one thing you will find more of than anything else in a salvage yard is scrap! It is all over the place, usually stocked in piles. The piles can contain all of one thing such as old brake wheels, sideframes, Etc. or they can be a combination of this and that.

(continued)

A LETTER TO 'YE OL' EDITOR

I read with interest the article in the last Hotbox about Airports on Model Railroads. The author, in talking about airlines mentioned that they can be included in any "past 1903" era layout. While technically this is true, I feel that I should point out that the first regularly scheduled airline in the U.S. began operation only in 1919, and that only about 5000 passengers were carried in the U.S. in 1926. Even by the 1930's there were less than 500 planes owned by domestic air services. So except for layouts in the 30's and later, airports would be surprizing, to say the least, on a layout. Let them land in farmer Jenkin's field! This would have certainly have been more commonplace!

As for the airplanes making the railroads less important, this too is open for debate. The fact of the matter is that in 1970, airlines carried only .18% of intercity freight traffic on a ton mile basis. The figure is actually less, since airlines have generally longer runs than railroads. Furthermore, much of this air traffic is new traffic which never moved on surface carriers. Since airlines carry mostly small, ver high value items, and since railroads carry large loads and mostly low value bulk commodities, little interchange occurs between these two carriers. Rather, much of the traffic interchange is between trucks & planes. (Incidently, much of the traffic diverted from other modes to air has been diverted from trucks, due to their similar transportation characteristics. Needless to say this is a point the airlines favor!) As was stated in the article, air service users are predominanty manufacturs of perishable items which need to be delivered very quickly. Let's face it gang, the railroads don't normally get this traffic. As for those important rush orders, e.g. meat, they don't put 20 tons of it on a plane. Rather rush orders are kept small because the air rates are so expensive.

Now, passenger traffic is a different story. Until the 50's railroads still had most of the passenger traffic. This is because business travelers used the train. Now business travel makes up a large portion of the for-hire passenger market, so when airlines switched to jets, they got the bulk of this service. So airlines are the culprit responsible for the demise of the passenger train (TN: And now we have Amtrak!). However, this also means you can have a good deal of rail-air interchange on your layout! If the airport is large enough, you can put in seperate station and maybe even run some special trains, e.g. to bring important diplomats, etc. from the outlaying airport to your Union Station in the heart of the big city. Use some modelers license if you don't have a city!

If you are in O or S scale, look around and see if you can find the old Bachmann "Plasticville" Airport Control Tower & Terminal & Airplane Hanger. I had an airport on the old Lackamoney Transportation Corperation's railroad using these structures, a Lionel airplane used as a flatcar load and one of those "old" cropdusters. I even had helicopter service! The runway, except for about 200 feet of it, was painted on the wall. This is a sneaky trick I know, but I was not about to give up any railroading room. In fact, if I had to scratchbuild everything as the author suggested in his article, the whole airport never would have gotten off the ground! (Pardon the pun)

Also, he suggests kitbashing a R.R. station to make an airport passenger terminal. Be careful with this! A Union Station type structure is OK but but your typical small town station, with it's extended roof line (to keep passengers standing on the platform from getting wet) is strictly railroady and shouls not be used.

Now a few comments about Mark Morgan's SP in New Mexico article. I liked

Kenneth Britton (cont'd)

it, but was surprised to find no mention of the Gadsden Purchase in it. As you probably know from your history books, this is a tract of land in southern Arizona and New Mexico, south of the Gila River. After the Mexican War in 1848 there was a dispute about the boundary between the two countries but more importantly there was a desire for a southern route for the proposed transcontinental railroad. Thus James Gadsden, a railroad promoter, and U.S. administrator to Mexico negotiated for the land and the U.S. bought it for a cool 10 million in 1854. Although this route cancelled the mountains to the north, the plans for a southern route were tabled with the outbreak of the Civil War. Eventually, the SP used the southern route, and all land on which SP tracks were built south of El Paso was part of the purchase.

I certainly hope some of these comments are of interest to our members. Despite my comments, I thought that these were 2 of the best and most interesting articles that I have seen in the HB, and I hope to see many more where that came from! In fact, they must have been good to get me off my apathetic & *\$% long enough to write this article!

Question & Answer column: Jim Talley

THE TAMR DOES ANSWER YOUR QUESTIONS...(IF WE CAN GET THEM!!)

In the Sept./Oct. Issue of the Hotbox, John McGreevy introduced the "Question & Answer Column" but it seems that we have a little problem... Now we don't think for one minute that the majority of you out there have NO questions to ask! I know that I myself haven't received any, John said he has a "few" and I don't know about the other 2 or 3 "experts".

Maybe if I explain how this new column works--You see, you send in questions you have that have been plaguing you about your pike or this organization and we will attempt to answer them to the best of our ability.

All you have to do send send your questions to: John McGreevy, 738 Camino Durango, 1000 Oaks CA 91360. John then attempts to answer them, if by chance he can't, he will send it along to one of his "experts" (I use the term loosely) to have it answered. After a question has been answered, it is sent directly to the HB Editor to be put in the next HB, so all the members will be able to see the question & answer to avoid possible repeat asking of the same questions.

Now that you realize that We realize that you have questions to ask there will be a flood of mail arriving soon for the Q&A Column, Right! I am sure it is worth the 13¢ stamp to find the solution to something that is puzzling you.

Mark Kaszniak (cont'd)

The condition of the scrap is quite stomach-turning to say the least, it ranges from rusted and corroded to grimy and oily to a combination of both.

A simple way to make piles of scrap without totally scrapping everything on your pike is to first make a rough shape of the pile with cardboard, then glue your "scrap" all over the top surface, then spraying the pile with rust, grime, dust, etc.

I hope this article has provided you with a few tips on making your own salvage yard. Just remember 3 basic rules in building it and it will look prototypical--(1) Nothing must look like it was intended to be placed there except for track, fence and offices. (2) Everything must look like it would be totally useless to a railroad, (3) Everything must look like it has been sitting in the same place for years and the ending of one scrap pile must look like the beginning of another like there was no starting or stopping point to the scrap.

CAJON PASS

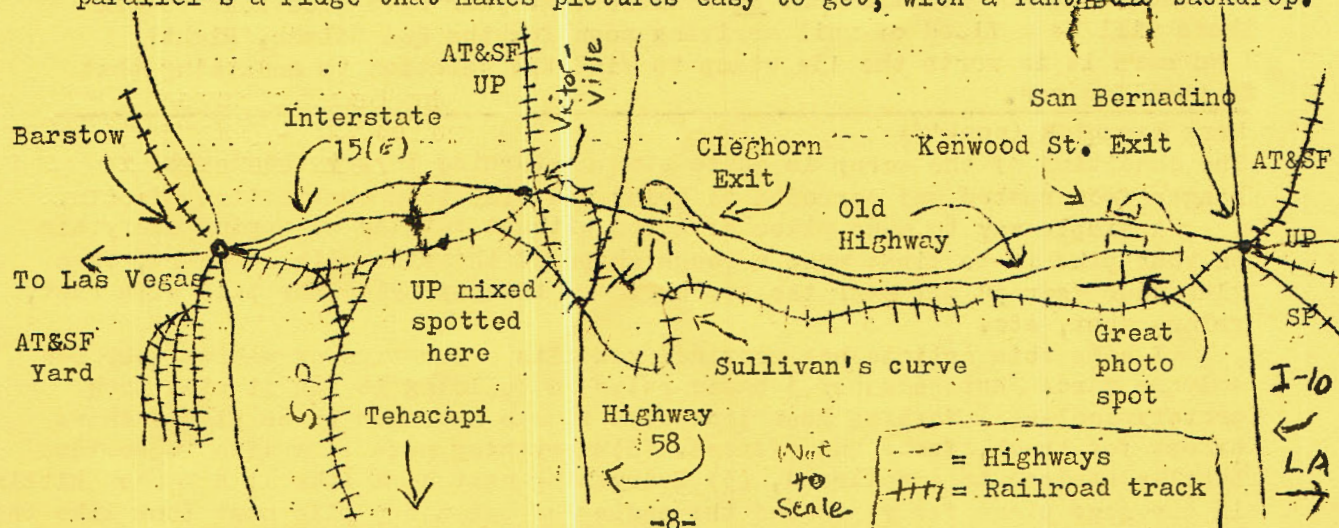
I have just come back from a very eventful trip to Cajon Pass and I will attempt to share it with you:

We got there about 10:00 AM, right off I-15 at the Kenwood Road Exit (See map). You wind up on old Highway 365--this is a stone's throw from track side but please never throw stones at trackside especially at passing trains. At 10:25, we saw a speeder stop and the worker turned a switch. Then a AT&SF work train backed toward us, downhill yet! A CF7 & GP35 powered this train, of the consist there was a big hook, a snowplow, and a very interesting crummy--it had a steam whistle on the roof--I waved and the conductor blew it. It really was a nice sound.

Then it was quiet. We decided to move eastward to Sullivan's Curve-- Take the Cleghorn Street exit off I-15 and follow the dead end road about 300 yards, till it turns to dirt, then park and you are at Sullivan's Curve. Here we waited and thus proving the theory that "all good things come to those who wait" we were rewarded with two AT&SF freight's. One of these freights contained your standard everyday--run of the mill type cars, the other was a TOFC freight. Of the mentioable notables of these freights were a U36C and a F45, they beat the SD45's we usually see thus giving us some variety. As for the rest of the trains we saw today, they were of the same type, oh yes, by the way all the trains are short and fast in this neck of the woods.

About the only outstanding thing we saw today was a UP mixed, with 17 containers, 10 various cars, and two UP coaches ahead of the hack. That was the train size now dig what was up front (in order) starting with the lead unit: SD40-2, #3125; DD40X #6938, SD40-2 #8062, DD40X #6932. That is a total of 18,200 available horsepower at the railhead. Needless to say, Bill, Chuck & myself chased this train across the desert at speeds that hit 85 mph. at times. We also managed to get about 40 pictures of this train before we were through.

If you ever go to Cajon Pass there are 3 very good photo locations as indicated by the map. Number one is Sullivan's Curve because of the location--there is plenty of warning in both directions as the trains approach, also you have a dual mainline and 2 different curves. Next is either of the two stretches of "old highway" depending on which way your coming. Number three is Kenwood's lead to the old highway--this parallel's a ridge that makes pictures easy to get, with a fantastic backdrop.



PRES. PACIFIC

Kitbashing an AHM Heavy Pacific into a B&O class P7 Presidential Pacific is an easy-to-do conversion, and it is well suited for a modelers first kitbashing project. Before we go into the details, however, let's examine the history of these interesting passenger locos.

In 1927, the B&O ordered 20 Pacifics from Baldwin. These engines, class P7, were based on the PRR's K4's class. Painted olive green with red and gold trim, they were named for the first 20 presidents of the U.S. They bore road numbers 5300 to 5319.

Sometime in the late 30's or early 40's they were rebuilt with Emerson water-tube fireboxes and dual cross-compound air compressors mounted on the pilot deck. They were repainted royal blue and were reclassified as P9b. (or P7e depending on the source, however, photo's of P9b's were seen in Railfan magazine, Winter 1974 & Winter 1976) They were all retired by the end of the 50's.

For those who desire the P9b version, you're in luck. Tyco has a Pacific in the right colors that is an exact duplicate except for one thing; the bell must be remounted on the smokebox front. For those who want the P7e Pacific you'll need the following parts:

One--Cal-Scale #190-244NP. B&O Plates.

One--Kemtron #390-34X. Side mounting Bell.

As for the engine, you have a choice here, AHM has 2 B&O Pacifics available. One is black and has a long-haul tender. The other is blue, gray and black and has the correct style tender. If you use the former version, you'll have to order a tender from AHM and repaint and letter it for B&O. (I used this engine by the way and have yet to order the tender) The latter engine presents a little bit of a mystery. According to Model Railroader, the B&O never had any engines in the blue, gray, black scheme. Yet according to B&O fans, they did. At any rate it is an attractive engine, and I recommend using it. Well, now to the actual kitbask.

First of all, remove the smokebox front from the engine. Then, cut off the bell and remove the headlight casting; be careful here, you'll need that headlight later. Okay, now that we've removed those 2 items, let's get to work. If any of the bell bracket or headlight bracket remains, gently scrap it off. Now, file down the back of the headlight casting 'till it resembles fig #1. Remember to remove the lens before you do this. Now glue the headlight on where the bell was, fig. #2

After the glue has set, spray the smokebox front black. This is important, if you spray the paint it will match the rest of the engine, if you paint it, it won't. After the paint is dry, put the lens in, see how much protrudes out the back, and trim it flush with the surface. Now remove the lens and paint the headlight housing silver. When dry, put the lens back in, if needed, glue the lens in, and put some contour putty on the back, if necessary.

After the putty dries, paint the back of the casting with engine black. Now we'll mount the B&O plates. To paint them do the following: First paint the first (not the sides) with engine black. Don't worry about covering the raised design with paint. When the paint is dry, rub the front over fine sandpaper a few times. As you can see, the paint is removed from the raised design thus exposing the gold color. To mount the plate, epoxy it into the smokebox front where the headlight hole is, fig. #3

Modelling: Greg LaRocca (cont'd)

The last step is to mount the new bell on the smokebox side. Paint the bell with engine black. When dry, drill a $\frac{1}{16}$ " hole approx. $\frac{1}{8}$ " above the handrail and $\frac{1}{8}$ " away from where the smokebox meets the boiler on the left side, fig. #4. Epoxy the bell in place.

Well, there you have it, B&O class P7 Presidential Pacific. By the way, if you were to take an AHM Heavy Mikado, modify the smokebox the same way, add an MDC Vanderbilt tender, number it in the 4000-series, you'd have a pretty nifty B&O Q4b Mike. Soon in the Hotbox I'll tell how to kitbash a F3 Phase IV/F5 unit.

FIGURES

Figure #1

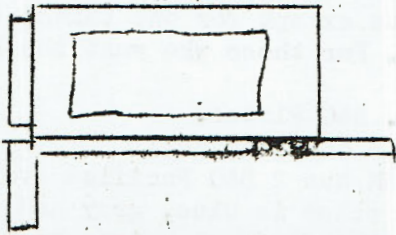


Figure #2

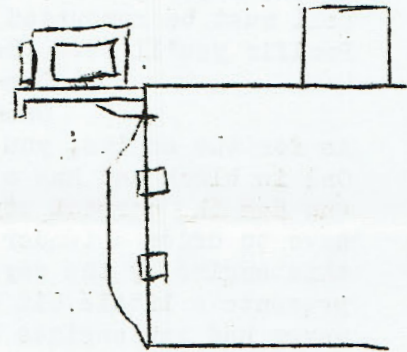


Figure #3

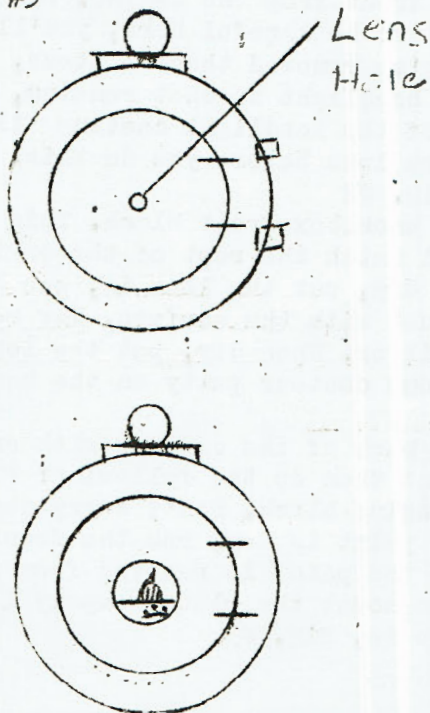
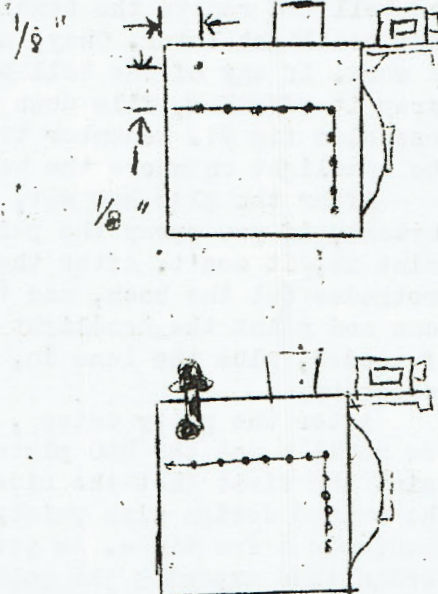


Figure #4



COLLAPSE OF A MODEL LAYOUT

Though I tried, I never succeeded in getting an article on my layout printed in the Hotbox (TN: Lock in last issue for article on John's pike). I finally got that article on my pike published but in the meantime I had an earthquake--California style. This jolt was not intended but how do you say no to an earthquake? Here is the whole story: I was forced to move my layout to a new location recently. My pike is an 8' by 8' with a walkway extending into the middle of it. Previously it had been attached to one wall and the rest was free standing. I found out that without the wall the layout would be freefalling! As soon as I removed it from the wall the legs began to wobble. As I was alone at the time, I tried to get in the middle and hold the thing up. Then came disaster. On one side the legs were holding up beautifully but on the other side they were about to give up the ship (pardon the expression).

The weak side fell to the floor, pushing me out of the way as it did while the strong side refused to fall but the pressure was on. First of all you can imagine with one side being on the floor and the other side standing that this was a situation does not do wonders for the plaster. A mini-San Andreas fault then went right through my newly completed lake and it was apparent that I had seen the last of my granite mountains. The agonizing part was having to break the good legs and thereby let the whole thing rest on the floor. Although I did get it off the wall, the layout certainly looked a shambles sitting on the floor. It was now separated into three major sections and had no legs. Good Heavens and other hard words like that were boiling in my mind. I was thinking: I hadn't even had a full operating session yet and two full years of work were beginning to pay off! Now I felt like forgetting the whole thing and starting over but how could I? I'm almost out of junior college and who knows how long it would take me just to get a new layout in the shape I got this one in. So after cooling down I decided if Conrail had a chance maybe I did. I'll try to rebuild!

There are lessons to be learned from everything and I learned one here. If you have a good possibility of moving your layout build a strong foundation (TN: Or build it in removable sections that can be taken apart and put back together again once it's moved). This collapse will set me back plenty. It isn't easy trying to rebuild during school. Think about this first. Second if out of nowhere you have to move your pike and don't think it will withstand the movement, shore up the legs and get some friends to help you move it. You must have some degree of patience. Finally if it does happen to you and you lose your pride and joy don't make decisions in a moment when your upset, think about it--paster can be redone/track replaced/maybe you can even improve a section. It can happen especially here in California where model railroads get full sized earthquakes!

THE YAKIMA VALLEY TRANSPORTATION COMPANY

is presently under construction, serving the fruit growers of Yakima. Timetables for the Orchard Line will be made known after its completion, late next year. Managing Director: Bryan Bechtold

THE PECATONICA & PONTOSUC RAILROAD

The President of the P&P R.R. is announcing that the railroad is now exchanging passes with interesting parties. Please contact the President of the P&P R.R. for details--Mark Kaszniak, 4818 W. George St., Chicago, IL. 60641

TEEN ASSOCIATION OF MODEL RAILROADING

OFFICER'S ADDRESSES

PRESIDENT Mark Soloman, 6 Batberry Branch, Casselberry FL 32707
AUDITOR John McGreevy, 738 Camino Durango, 1000 Oaks, CA 91360
SECRETARY Douglas Johnson, Bau Hall Box 347A, Valparaiso, IN 46383
TREASURER Ray Hakim, 21 Clauss Ave., Paramus, NJ. 07652
ASSISTANT SECRETARY FOR MEMBERSHIP--
Brian Angeli, State Line Drive, Pownal, VT. 05261

HOTBOX STAFF

Editor Tim Vermande, 51528 Pond St., South Bend, IN 46637
Advertising Mark Kaszniak, 4818 W. George St., Chicago, IL. 60641
Publisher Andrew Dick, 15 Crescent Drive, Convent Station, NJ 07961
Circulation Edward Shelby, Route 2 Box 52, Palisade, TENN. 38478
Typing Mark Kaszniak (above)

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TAMR HOTBOX

51528 Pond St.

South Bend, IN 46637

AM

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CHICAGO



United States 13c

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FIRST CLASS MAIL

606

This HOTBOX never issued to general membership. Contents split up and distributed in later issues.